

## AGENDA

### AMBLER BOROUGH PLANNING COMMISSION

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**I. CALL TO ORDER**

**May 26, 2026 (7:00 PM)**

**II. ROLL CALL**

**III. APPROVAL OF: April 28, 2026 minutes**

**IV. CORRESPONDENCE / MEETINGS / COORIDINATION AND OUTREACH**

**V. OLD BUSINESS:**

1. Input is requested on updating the 2013 Borough Comprehensive Plan. Draft goals and proposed themes / outline to be discussed.
2. Recommend to Council Ordinance amendments to require recreation area or open space for all new developments or subdivisions. This would also include provisions for a fee in lieu of in the event recreation area or open space is not feasible that can be put towards existing park improvements. *Motion Requested*

**VI. NEW BUSINESS**

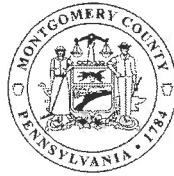
1. Review "Ambler Crossings Phase II (South Chestnut Street) preliminary and final land development application proposing construction of a 92-unit residential apartment building with associated driveways, parking, utilities, and stormwater management design. *Motion Requested*

**VII. ADJOURNMENT**

**MONTGOMERY COUNTY  
BOARD OF COMMISSIONERS**

Jamila H. Winder, Chair  
Neil K. Makhija, Vice Chair  
Thomas DiBello, Commissioner

[www.montgomerycountypa.gov](http://www.montgomerycountypa.gov)



**Montgomery County  
Planning Commission**

Montgomery County • PO Box 311  
Norristown, PA 19404-0311

610-278-3722  
[planning@montgomerycountypa.gov](mailto:planning@montgomerycountypa.gov)

Scott France, AICP  
Executive Director

April 24, 2026

Mr. Kyle Detweiler, Borough Manager  
Borough of Ambler  
131 Rosemary Avenue  
Ambler, Pennsylvania 19002

Re: MCPC #26-0078-001  
Proposal Name: Subdivision Land Development Ordinance Amendment  
Borough of Ambler

Dear Mr. Detweiler:

We have reviewed the above-referenced subdivision and land development ordinance amendment in accordance with Section 505 of Act 247, "The Pennsylvania Municipalities Planning Code," as you requested on March 26, 2026. We forward this letter as a report of our review.

## **BACKGROUND**

The Borough of Ambler is proposing to amend the Subdivision and Land Development Ordinance (SALDO) to establish a dedication of recreation land requirement. The requirement would affect any development with at least two dwelling units, as well as nonresidential and mixed-use developments. Under the proposed regulations, applicants would be required to set aside recreation land and offer it for dedication to the borough. The proposal also offers alternatives of providing a fee in-lieu of the requirement, improving an existing borough recreation site, privately preserving land, or using a combination of these options.

## **COMPREHENSIVE PLAN COMPLIANCE**

### **BOROUGH COMPREHENSIVE PLAN**

The borough's 2013 *Comprehensive Plan Update* builds upon the 2001 *Comprehensive Plan* by reiterating the goals of the original document. The primary parks and recreation goal included in the 2013 update is the intent to "enhance Ambler's park facilities for residents to enjoy and seek opportunities to connect with the future Wissahickon Trail." The proposed regulation would provide an

opportunity for the borough to advance this goal through continual improvements to the parks and recreational amenities in the community.

## COUNTY COMPREHENSIVE PLAN

The Comprehensive Plan for Montgomery County, *Montco 2040: A Shared Vision*, sets forth a goal to "provide more opportunities for residents to exercise and have healthy lifestyles" (pg. 52). The proposed regulation's ongoing implementation will continue to support and advance this objective.

## RECOMMENDATION

The Montgomery County Planning Commission (MCPC) generally supports the applicant's proposal; however, in the course of our review we have identified the following item that the borough may wish to consider prior to adoption. Our comments are as follows:

## REVIEW COMMENTS

### APPLICABILITY

Different types of dwelling units generally have different household compositions; for example, apartments in Montgomery County typically have fewer individuals and fewer school-aged children per unit.<sup>1</sup> With this factor in mind, the borough may wish to consider modifying Section 22-309.B. to provide different requirements based on the type of dwelling unit proposed.

If the borough is interested in addressing different types of dwelling units within this section, MCPC provides guidance in our *Model Subdivision and Land Development Ordinance*. This document recommends the following rates of recreational land:

- Single-family detached: 2,500 square feet per unit
- Two-family and single-family attached: 2,000 square feet per unit
- Multifamily apartments: 1,500 square feet per unit
- Nonresidential: 1% of the gross acreage

## CONCLUSION

We wish to reiterate that MCPC generally supports the proposal but we believe that our suggested revisions will better achieve Ambler's objective of ensuring adequate recreational facilities are provided as part of future land developments.

Please note that the review comments and recommendations contained in this report are advisory to the municipality and final disposition for the approval of any proposal will be made by the municipality.

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<sup>1</sup> *Who Are Our Neighbors? A Demographic Profile of the Occupants of Montgomery County's Residential Development*. Montgomery County Planning Commission, 2025.

Should the governing body adopt this proposed Subdivision and Land Development ordinance amendment, Section 505 of the Municipalities Planning Code requires that we be sent an official copy within 30 days.

Sincerely,

A handwritten signature in black ink that reads "Timothy Konetchy". The signature is written in a cursive, slightly slanted style.

Timothy Konetchy, AICP, Senior Community Planner  
[Timothy.Konetchy@montgomerycountypa.gov](mailto:Timothy.Konetchy@montgomerycountypa.gov) - 610.292.4917

- c: Glenn Kucher, Assistant Borough Manager & Code Enforcement Officer
- John Oswald, Chair, Borough Planning Commission
- Al Comly, Vice Chair, Borough Planning Commission
- Carol DiPietro, Secretary, Borough Planning Commission
- Jessica Buck, District Manager, MCCD

**AMBLER BOROUGH  
ORDINANCE NO. 2026- \_\_\_\_\_**

**AN ORDINANCE OF AMBLER BOROUGH, MONTGOMERY COUNTY,  
PENNSYLVANIA, AMENDING CHAPTER 22, “SUBDIVISION AND LAND  
DEVELOPMENT” AT SECTION 309 – “DEDICATION OF RECREATION LAND”;**

**WHEREAS**, Ambler Borough (“Borough”) is a Borough duly organized and existing pursuant to the applicable laws of Pennsylvania; and

**WHEREAS**, pursuant to the Borough Code of the Commonwealth of Pennsylvania, the Council of Ambler (“Council”) has the authority to enact and amend provisions of the Ambler Borough Code (the “Code”) at any time it deems proper; and

**WHEREAS**, the authority to enact such ordinance is granted to the Borough under the Pennsylvania Borough Code, including but not limited to its general police powers; and

**WHEREAS**, the Borough Council is desirous of amending the Borough Code to update Chapter 22 – “Subdivision and Land Development” at Section 309 – “Dedication of Recreation Land” for the health, safety, and general welfare of the Borough and the inhabitants thereof.

**NOW THEREFORE**, be it **ORDAINED AND ENACTED** by the Ambler Borough Council as follows:

**Section 1.** The Code of Ordinances of Ambler Borough is amended to include Chapter 22 – “Subdivision and Land Development” Section 309 – “Dedication of Recreation Land” as follows, with the stricken text indicating the removed portion of the Code and the underlined text indicating the added portion of the Code:

“§22-309. Dedication of Recreation Land

- A. Intent. In order to continue to provide adequate recreational opportunities for current and future residents of the borough, all new development shall be required to offset the anticipated impact on existing facilities by providing open space and recreation areas. The provisions of this section aim to maintain and expand recreational facilities within the community.
- B. Applicability. Suitable recreation land shall be provided for each subdivision or land development, which shall be offered for dedication to the borough in accordance with the following:
  - 1. Residential uses and mixed land uses containing two (2) or more residential dwelling units shall offer for dedication 3,000 square feet of land dedicated to park and recreation areas per dwelling unit.
  - 2. Nonresidential and mixed land uses shall offer for dedication 3,000 square

feet of area dedicated to park and recreation areas per 5,000 square feet of nonresidential gross floor area or portion thereof.

C. Conformance to existing plans. The land to be used for park and recreational facilities shall be in accordance with the principles and standards contained in the duly adopted *Borough of Ambler Comprehensive Plan, Open Space Plan Update* and *Ambler Borough Parks: Vision for the Future*.

D. Park and recreational land or facilities standards.

1. The land shall be well-drained and suitable for public access.
2. The land may be for active recreation (e.g., formalized athletic fields or courts), passive recreation (e.g., walking, hiking, bird watching, etc.), or a combination thereof.
3. The average slope of the land shall be less than 6%. Notwithstanding, natural topography can remain undisturbed provided that the recreation land is suitable for active recreational purposes by a wide range of users.
4. If an existing park or trail is contiguous, the land dedicated shall connect to the existing park or trail.
5. Open space shall be connected by sidewalk, crosswalk, and/or trails to all buildings on-site and to the public sidewalk along the street.
6. The land shall not be used as stormwater management facilities or stormwater basins, unless the stormwater management facility is designed in a naturalized manner that provides recreational or aesthetic value, such as a rain garden or vegetated swale.
7. Open space may include land within utility corridors only if the utility companies having legal rights to these corridors do not prohibit their use for such purposes.

E. Alternatives to the development of recreational land. Upon agreement of both Borough Council and the applicant, the applicant may pursue the following alternatives:

1. Fee in lieu.
  - a. Where Borough Council and the applicant agree that a fee is to be contributed in lieu of the provision of land for recreation on the development site as required by this section. The fee in lieu of dedication shall be set forth in the Fee Schedule as duly adopted by Borough Council from time to time.

- b. Any fee in lieu of dedication which is collected by the borough shall be used only for the purpose of providing park and recreational facilities within Ambler Borough. Upon receipt, the fee paid by the applicant shall be deposited in an interest-bearing account designated for Park and Recreation. Funds from the account may only be expended on specific park and recreation facilities approved by Borough Council.
2. Improvements to other recreation sites. The applicant may, when approved by Borough Council, purchase additional land for existing or proposed borough parks, or construct recreational facilities on existing borough parkland that is readily accessible to residents of the proposed development. The value of such improvements shall be comparable to the value of the fee in lieu of recreation land, in accordance with the provisions of this section.
  3. Private preservation of land. The applicant may reserve land in the amount required under this chapter. The land shall meet all the standards in this section, be available for use by the general public, and managed and maintained in conformance with any adopted park and recreation plan, and any ordinances covering maintenance requirements for private ownership of common elements.
  4. A combination of land dedication, fee-in-lieu payment and/or alternative approaches as listed herein may be pursued, as reflected in a written agreement between the applicant and Borough Council.”

**Section 2.** All provisions of the Code of Ordinances of Ambler Borough unaffected by this Ordinance are declared to be in full force and effect. Any provisions of the Code of Ordinances of Ambler Borough inconsistent with the provisions of this Ordinance are hereby repealed to the extent of the inconsistency.

**Section 3.** The provisions of this Ordinance are declared to be severable and, should any portion, part or provision of this Ordinance be found by a court of competent jurisdiction to be invalid, unenforceable or unconstitutional, the Council hereby declares its intent that the Ordinance shall have been enacted without regard to the invalid, unenforceable, or unconstitutional portion, part or provision of this Ordinance.

**Section 4.** This Ordinance shall become effective immediately upon its enactment by the Council of Ambler Borough.

**DULY ENACTED** on this \_\_\_\_\_ day of \_\_\_\_\_, 2026.

**AMBLER BOROUGH COUNCIL**

By: \_\_\_\_\_

Elizabeth M. Iovine, Council President

**ATTEST:**

\_\_\_\_\_

Jeanne Sorg, Mayor

APPLICATION FOR SUBDIVISION &  
LAND DEVELOPMENT REVIEW  
BOROUGH OF AMBLER  
131 Rosemary Avenue  
Ambler, PA 19002  
(215) 646-1000  
FAX (215) 641-1355



Development Name: Ambler Crossings - Phase II

Site Location: South Chestnut Avenue

Legal Property Owner Name: ELU Mid-Atlantic Ambler, LLC

Address: 201 S. Maple Avenue, Suite 100, Ambler, PA 19002

Phone: 302-740-6429 Fax: \_\_\_\_\_

E-Mail Address: jzaharchuk@summitrealtyadvisors.com

**Contact Person** (to whom all correspondence will be sent):

Name: John Zaharchuk

Address: 201 S. Maple Avenue, Suite 100, Ambler, PA 19002

Phone: 302-740-6429 Fax: \_\_\_\_\_

E-Mail Address: jzaharchuk@summitrealtyadvisors.com

Interest of applicant is:  Owner  Equitable Owner  Other-explain \_\_\_\_\_

**Zoning Information:**

District Redevelopment Overlay District

Tax Parcel Number **01-00** -01157-30-9

Block \_\_\_\_\_ Unit \_\_\_\_\_

Permitted Density 35 DU/Ac. - (20 DU/Ac. proposed)

Total Area 4.61 acres

**Type of Review Requested:**

Subdivision Plan  Land Development  Conditional Use  Amendment

**Type of Plan:**

Sketch  Preliminary  Final

**Type of Submission:**

New Proposal  Revised Prior Submission

**Statement of Intent:** Construction of a 92-unit residential apartment building with associated driveways parking, utilities and stormwater management design.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Land Use/Proposed Number of Lots/Units Intended Use(s):**

- Residential      1 lot/92 units      Apartments
- Commercial      \_\_\_\_\_
- Industrial      \_\_\_\_\_
- Office      \_\_\_\_\_
- Other      \_\_\_\_\_

**Please complete the following items which are applicable to your project:**

**Attorney's Name:** \_\_\_\_\_

**Address:** \_\_\_\_\_

**Phone:** \_\_\_\_\_ **Fax:** \_\_\_\_\_

**E-Mail:** \_\_\_\_\_

**Engineer's Name:** Holmes Cunningham Engineering - Robert Cunningham, P.E.

**Address:** 409 E. Butler Ave., Unit 5 Doylestown, PA 18901

**Phone:** 215-586-3330 **Fax:** \_\_\_\_\_

**E-Mail:** rob@hcengineering.net

I certify that the plans submitted comply with the requirements of Chapter 22 of the Ambler Borough Code of Ordinances.

**Plan submitted by:** ELU Mid-Atlantic Ambler, LLC (please print)

**Applicant's Signature:** \_\_\_\_\_

**Application Fee:** \$ 1,990       Attached       Under separate cover

**Escrow Amount:** \$ (TBD by Borough)       Attached       Under separate cover

Ambler Borough Fee Schedule (non-refundable)  
Checks should be made payable to "Ambler Borough"

**RESIDENTIAL SUBDIVISION/LAND DEVELOPMENT: Application Fee**

Preliminary Plans	\$150.00 plus \$20.00 per unit/lot or each additional 1,000 S.F.
Final Plans	\$150.00 plus \$20.00 per unit/lot or each additional 1,000 S.F.

**NON-RESIDENTIAL SUBDIVISION/LAND DEVELOPMENT: Application Fee**

Preliminary Plans	\$150.00 plus \$20.00 per unit/lot or each additional 1,000 S.F.
Final Plans	\$150.00 plus \$20.00 per unit/lot or each additional 1,000 S.F.

**ESCROW FUND: Established based on the following calculations**

Minor Subdivisions/Land Development (4 lots or fewer)	AT COST
Major Subdivisions/Land Development (5 lots or greater)	AT COST

**CONDITIONAL USE HEARING (Council):**

Residential \$500.00; Non-Residential \$1000.00 plus costs.







Drawing No.  
**C0.2**

DATE	9/12/2023
SCALE	1"=40'
DESIGN	RS
SHEET	3 of 25



**AMBLER CROSSINGS - PHASE II**  
 MAPLE AVENUE AND CHESTNUT AVENUE  
 TMP # 01-00-0157-30-0  
 AMBLER BOROUGH, MONTGOMERY COUNTY, PENNSYLVANIA

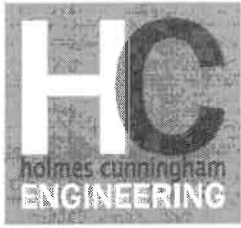
**AERIAL PLAN**

DATE	9/12/2023
SCALE	1"=40'
DESIGN	RS
SHEET	3 of 25

Hobbes Consulting LLC  
 409 E. Butler Ave., Unit 5  
 Doylestown, PA 18901  
 (610) 586-3300  
 www.hobbesconsulting.net







Kristin Holmes, P.E., LEED AP  
Robert Cunningham, P.E., LEED AP

March 20, 2026

Kyle Detweiler, Borough Manager  
Borough of Ambler  
122 E. Butler Avenue  
Ambler, PA 19002

**RE: Ambler Crossings – Phase II, ELU Mid-Atlantic Ambler, LLC  
Preliminary/Final Land Development Application  
TMP#: 01-00-01157-30-9  
Ambler Borough, Montgomery County, PA  
HCE Project No.: 1845**

Dear Mr. Detweiler:

We are in receipt of a review letter for the above-referenced project. Below please find responses to each of the comments contained in this letter.

*Review Letter from Gilmore & Associates, Inc. dated October 16, 2025.*

Zoning (Chapter 27)

1. §27-412.4.C(3) – The lighting system shall be capable of extinguishing offending light sources between 11PM and dawn to mitigate glare.  
**Response: A note has been added to the Lighting Plan on Sheet C6.1 stating this requirement.**
2. §27-412.4.D(2) – “Lighting standards in parking areas shall be placed a minimum of five feet outside of curb.” It is noted that the provided Light Pole Foundation detail indicates a distance to curb or edge of paving of 18 inches. The detail shall be revised.  
**Response: Light standards have been provided on a 30” pedestal as an option listed by the code. Additionally, a note has been added to the Lighting Plan on Sheet C6.1 stating this requirement.**
3. §27-2102 and §27-2703.E(4)(e)(3) – Parking spaces are to be removed from the Phase 1 parking area. Verify that adequate parking will continue to be provided in Phase 1. If spaces are shared between Phases 1 and 2, it shall be noted on the plans. It is noted that Phase 1 shares spaces with the Boiler House. The requirements of §27-2703.E(4)(e)(3)(a) and (b), associated with shared parking, shall be addressed.  
**Response: A Shared Parking Table has been added to Sheet C1.0 to enumerate the spaces shared between the Boiler House, Phase I, and Phase II. A Parking Study has been included with this submission.**
4. §27-2703.E(4)(e)(3) – A parking generation study is required. The study shall include analysis of the proposed shared parking arrangements for the Boiler House, and Ambler Crossings phases I and II.  
**Response: A Parking Study has been included with this submission.**

5. §27-2703.E(3)(a) – A TOD shall be permitted with the RO Redevelopment Overlay District when authorized as a conditional use by Borough Council.  
**Response: Acknowledged.**
6. §27-2703.E(4)(g), §27-413 & §27-2703.E(4) – These sections of the ordinance provide the conditional use standards. The applicant should demonstrate compliance with these standards.  
**Response: On Plan Sheet C1.0, the zoning data table is revised to show compliance with the regulations. Additionally, a General Note 7 is provided to reference the architectural plans.**
7. §27-2703.E(3)(b)(1) – Dwelling units shall be located on the second story and above.  
**Response: Acknowledged.**
8. §27-2703.E(3)(c) and §20-2703.E(4)(a)(2) – A non-residential use is required along with the residential use within the TOD. The plan does not appear to include a non-residential use.  
**Response: Newly adopted ordinance states that “Any building or portion thereof that does not have a frontage on Butler Avenue or Main Street may include a residential use on the ground floor, provided that such complies with § 27-2703.E.3.(c)1. and provided that architectural elements that provide visual interest are provided along any façade abutting a public right-of-way. Examples of architectural treatments to comply with this requirement include: front stoops, porches, awnings, balconies, recessed entranceways, courtyards, trellises with plantings, or other similar architectural features.” The proposed development meets this requirement.**
9. §27-2703.E(4)(a)(1) – A TOD use must be located with 800 feet of an active or proposed commuter rail station. The 800-foot requirement shall be measured from the nearest property line of the TOD to the nearest edge of the passenger platform of the commuter rails station. This distance shall be documented on a plan.  
**Response: A Plan Sheet C0.2 has been provided showing the distance to the nearest edge of a passenger platform of the SEPTA Ambler Train Station. Additionally, this requirement has been added to the Zoning Data Table.**
10. The subject lot is an irregular shaped flag lot with limited frontage on South Chestnut Street. We defer to the Ambler Code Official regarding the following requirements that are based upon frontage given the configuration of the lot.
  - a. §27-2703.E(4)(b)(3) – The minimum frontage occupation is 90%.  
**Response: The proposed plan has been reviewed by the Ambler Code Official.**
  - b. §27-2703.E(4)(b)(5) – The build-to line is 15 feet.  
**Response: Per Borough Ordinance “1140 Amending the Redevelopment Overlay District,” the application provides landscaping between the building and sidewalk and thus the build-to line is increased to 17.9 feet.**

11. §27-2703.E(4)(c) – Compliance with building design standards is not included in the scope of our review and will be reviewed by others.

**Response: Acknowledged.**

12. Update the Zoning Data Table on Drawing C1.0 (sheet 3 of 19) to reference the current minimum lot area, minimum lot width, build-to line, etc. Refer to Ordinance No 1140.

**Response: On Plan Sheet C1.0, the Zoning Data Table has been revised.**

Subdivision & Land Development (Chapter 22)

13. §22-305.4.A and §22-306.A(2) – The plan shall include existing features within 400 feet of any part of the land being developed.

**Response: A plan sheet C0.2 has been added to the plan with an Aerial photograph to show off-site features.**

14. §22-305.4.B(13) and §22-306.A(2)(c) – An access easement is required between the Phase I and Phase II parcels since the property is under different entities. Show an access easement on the plans and provide the easement documentation to the Borough.

**Response: Easement exhibits have been included as supplementary plans and listed on the cover sheet. An access easement exhibit is included with this submission.**

15. §22-308.C – The applicant is requesting preliminary and final plan review. Simultaneous consideration of preliminary and final approval is at the discretion of Borough Council when a waiver is requested by the applicant.

**Response: A waiver is requested to allow preliminary and final plan review and approval.**

16. §22-310 – Traffic Impact Study

- a. All individual turning movement of roadways and/or intersections showing a level of service below C shall be considered deficient, and specific recommendations for the elimination of these problems shall be listed. There are multiple movements with a LOS D.

**Response: So noted. Justification of deficient movements has been included in the revised study.**

- b. The signal timings are inconsistent between the existing conditions, base conditions and projected conditions. If timings need to be adjusted, provide a discussion in the report for the following intersections:

- i. Main Street and Butler Avenue
- ii. Maple Avenue and Butler Avenue
- iii. Spring Garden Street and Butler Avenue

**Response: The timings in the existing conditions were optimized per PennDOT guidelines and the optimized timings were utilized in the base and projected conditions.**

- c. If any timing adjustments are needed to mitigate the development's traffic, the applicant will be responsible for permitting and furnishing the changes.

**Response: So noted, however, timing adjustments were not necessary for mitigation.**

- d. Revise the AM Peak Hour capacity analyses for Main Street and Butler Avenue. The northbound right heavy vehicle percentage does not match the traffic counts.  
**Response: This has been revised accordingly within the revised Traffic Study.**

- e. Revise the capacity analyses for Spring Garden Street and Butler Avenue. The signal plan indicates the northbound lane width is 9 feet while the analyses show 12 feet.  
**Response: This has been revised accordingly within the revised Traffic Study.**

- f. Revise the PM Peak Hour capacity analyses for Spring Garden Street and Butler Avenue. The northbound right heavy vehicle percentage does not match the traffic counts.  
**Response: This has been revised accordingly within the revised Traffic Study.**

- g. Table 2 recommendations indicate that STOP signs and associated pavement markings will be provided at the accesses. However, these are not shown on the plans. Provide the stop signs on the plans.  
**Response: The STOP signs and associated pavement markings proposed at the accesses have been added to the plans.**

17. §22-313.1.B – A 10' x 10' illuminance grid (point-by-point) plot of maintained horizontal footcandles overlaid on the site plan shall be provided.  
**Response: 10'x10' grid is provided.**

Engineering Standards (Chapter 22, Appendix A)

18. §22A-108, 109, & 110 – Provide documentation that water and sewer capacity exists and will be provided. If any easements are required, depict them on the plans.  
**Response: Acknowledged.**
19. §22A-111 – Provide documentation the MCCD has reviewed and approved the plan.  
**Response: Acknowledged.**

Landscape Planting Requirements (Chapter 22, Appendix B)

20. §22B-100.2.2.A – “There shall be no more than 10 contiguous parking stalls in a row without a planting island.” It is noted that a waiver is requested for relief from this requirement.  
**Response: As stated, a waiver is requested from this requirement.**
21. §22B-100.2.2.C – “Each planting island shall contain one shade tree plus shrubs and/or groundcover to cover the entire area.” It is noted that shrubs and/or ground cover to cover

the entire area within the planting islands have not been provided. The plan shall be revised.

**Response: All planting islands contain shade trees where feasible. Two (2) planting islands conflict with the underground stormwater management system. Shrubs are installed in these locations in lieu of shade trees.**

22. §22B-100.2.2.E – “The placement of light standards shall be coordinated with the landscape design to avoid conflict with the operation of light fixtures.” It is noted that lights standards are not shown on the Landscape Plan nor is the proposed landscape depicted on the Lighting Plan. Each plan shall be revised to demonstrate a minimum distance of 10 feet between plantings and light fixtures.

**Response: Light standards and lighting templates have been shown on the Landscaping Plan, Sheet C6.0.**

23. §22B-100.3.3.C – “Trees shall be planted a minimum distance of 15 feet from overhead utilities and six feet from underground utilities.” It is noted that the plan shows existing overhead utility lines transecting the site. It is unclear if these lines will remain or be re-routed. Currently, proposed trees are less than 15 feet from the overhead lines. Clarification shall be provided.

**Response: The existing overhead lines are to be removed, and have been removed from the landscaping plan.**

24. §22B-100.4.3.G – “For every 100 linear feet of property line to be buffered, there shall be one canopy tree and two ornamental trees.” It is noted that a partial waiver is requested with the justification that the property is surrounded by the railroad/retaining wall, the Ambler Crossing Phase I development and existing vegetation.

**Response: Acknowledged. A partial waiver is requested.**

25. §22B-100.7.B(1) – The landscape plan shall provide a north arrow and zoning district designations for the site and adjacent properties.

**Response: On Plan Sheet C6.0, the north arrow and zoning district designations are shown on the plans.**

#### Stormwater Management (Chapter 26. Part 4)

26. §26-422 – The applicant shall provide documentation that any permits required by other government agencies are in place. An NPDES permit is required.

**Response: Acknowledged; outside agency permits will be provided upon receipt.**

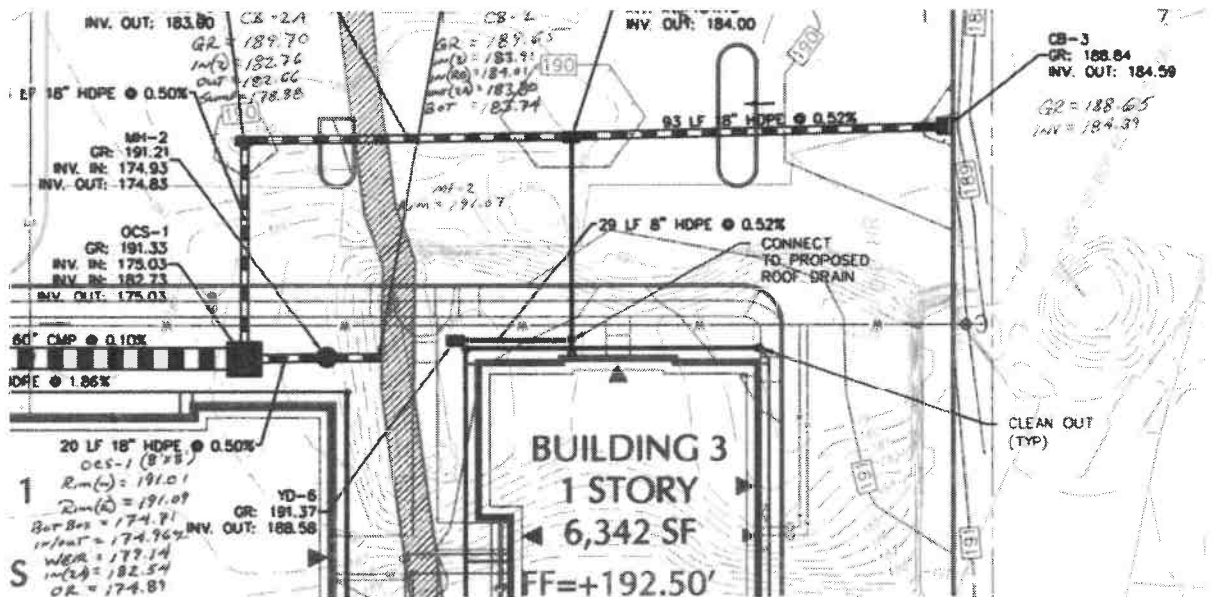
27. §26-432.B – Stormwater runoff peak discharge from sites with drainage areas greater than 2 acres shall be calculated using a method based on the NRCS SCS method. The provided analysis uses the rational method. A method from Table 26-432.1 shall be used.

**Response: The stormwater calculations have been updated to use the NRCS SCS method.**

28. §26-432.C – All calculations using the SCS method shall use the design storm depths found in SWM Ordinance Appendix A Table A-1.

**Response: The design storm depths from Appendix A Table A-1 have been used for the SCS stormwater calculations.**

29. §26-432.J – The design of storm water facilities intended to meet the standards of Ambler’s Ordinance shall be verified by routing the design storm hydrograph through the facilities. The design storm hydrograph shall be computed using an acceptable calculation method (Table 26-432.1) the produces a full hydrograph. The provided analysis uses the rational method. A method listed in Table 26-432.1 shall be used.  
**Response: The stormwater calculations have been updated to use the NRCS SCS method, which produces full hydrographs in accordance with the ordinance.**
30. §26-428 and §26-433.A.1 – The applicant indicates that the site subsurface is contaminated and that infiltration is not permitted. The applicant has designed the stormwater facilities to not infiltrate due to existing subsurface contamination.  
**Response: Correct; due to the subsurface contamination, infiltration is not permitted by outside agencies. In addition, the Stormwater Management Ordinance permits an exemption from groundwater recharge requirements in “hotspots” and areas of contamination. Stormwater runoff volume control will be achieved with a Managed Release Concept (MRC) facility as noted below.**
31. §26-429 – The applicant is proposing a Managed Release Concept (MRC) stormwater BMP to address the volume control requirements.  
**Response: Acknowledged. Because infiltration is not permitted, stormwater volume is proposed to be managed through use of an MRC facility in accordance with PADEP guidance.**
32. §26-441.A – The applicant proposes to discharge stormwater to a neighboring property (Crossings Phase I). An easement is required to allow this.  
**Response: On Plan Sheet C1.0, a stormwater easement is proposed to allow for stormwater to discharge on to the Phase I property.**
33. §26-441.B – The owner shall provide the Borough an easement to perform inspections and maintenance of the proposed stormwater management facilities.  
**Response: On Plan Sheet C1.0, note 8 is provided.**
34. The storm sewer connection at existing Crossings Phase I inlet CB-3 conveys stormwater to the Phase I UG Detention Basin-1. It is noted that the Phase I system was designed to contain the entire 24-hour, 100-year design storm volume due to tailwater and capacity constraints within the existing stone arch culvert. The stormwater management report shall include analysis demonstrating that the proposed Phase II system will function as intended and not impact the function of the Phase 1 system or the existing stone arch culvert in any way. Either the proposed design shall account for maximum tailwater conditions at the Phase I UG Detention Basin-1 or the proposed system shall be sized to contain the 100-year storm volume.  
**Response: While we agree with the consideration of tailwater from the Phase I basin and stone arch culvert system, we do not believe that tailwater affects the proposed Phase II basin. The below screenshot depicts an as-built markup of the Phase I site:**



The Phase II system proposes to connect to CB-3 at invert elevation 184.65. This is 4.62 feet above the top of the 60" CMP header pipe. Even if tailwater were considered to have filled the Phase I system, that condition would not affect the operation of the detention facilities on the Phase II site.

35. Provide documentation that the UG SWM facility will support fire truck outrigger loading (43,200 lb on 24-inch x 24-inch outrigger = 75 psi).

**Response:** The subsurface stormwater facility in the northeastern parking lot has been relocated to be opposite the building and largely under car parking areas, such that a fire truck would not set up outriggers in that location. The subsurface stormwater facility to the southwest of the building has been located solely under car parking areas.

36. General note 6 on Drawing C1.0 should be revised to note the proposed stormwater discharge location.

**Response:** On Plan Sheet C1.0, general note 6 has been revised.

### General

37. The Applicant should obtain all required approvals, permits, declarations of restrictions and covenants, etc. (e.g., PADEP, PennDOT, MCPC, Montgomery County Conservation District, Fire Marshal, etc.). Copies of these approvals and permits should be submitted to the Borough and our office.

**Response:** Acknowledged.

38. It is the applicant's responsibility to verify the property meets residential health standards. The applicant shall provide documentation that the site meets the Statewide Health Standards for residential development.

**Response:** Acknowledged.

39. Details regarding the ownership of the existing 20-foot-wide access easement from South Chestnut Street shall be provided.  
**Response: This existing easement is owned by the USEPA for access to maintain the existing material piles on the Caddick sites. Documentation has been included with this submission.**
40. The retaining wall detail shall include a note regarding guards/fall protection where required by the building code.  
**Response: On Plan Sheet C1.2, the retaining wall detail has been revised.**
41. We recommend that emergency services review the plans and provide any pertinent input.  
**Response: Acknowledged.**
42. Accessible routes and features to the extent required by building codes shall be added to the plans.  
**Response: On Plan Sheet C2.1 and C2.2, accessible routes are shown.**
43. Dimension all curblines radii in the parking area.  
**Response: On Plan Sheet C1.1, radii are dimensioned.**
44. Provide 5-scale ADA ramp details for all ramps including detectable warning surfaces, dimensions, spot elevations and slopes to verify ADA compliance and constructability.  
**Response: On Plan Sheet C2.1 and C2.2, ADA ramp details have been provided.**
45. Provide sidewalk on the north side of the building to provide a connection between the sidewalk on the east and west sides of the building.  
**Response: On Plan Sheet C1.0, a sidewalk connection has been added.**
46. Wheel stops should be provided for all parking spaces that are perpendicular to sidewalks that are less than 6 feet wide to limit vehicle overhang.  
**Response: On Plan Sheet C1.1, parking spaces abutting sidewalks are provided with wheel stops.**
47. Show the location of any utilities/utility poles in the eastern parking area that will be relocated.  
**Response: The existing utilities and poles in the eastern portion of the site are proposed to be removed, and the plans have been updated to exclude them from the plan view in proposed conditions.**
48. We have the following comments regarding signage and pavement markings:
- a. Show all proposed signage locations (i.e. stop signs, handicap parking signage, etc.).  
**Response: On Plan Sheet C1.1, proposed signs are shown.**
  - b. The parking space sizes are inconsistent between details sheet and plan sheets. Revise the plans for consistency.  
**Response: On Plan Sheet C1.2, the pavement markings detail has been revised.**

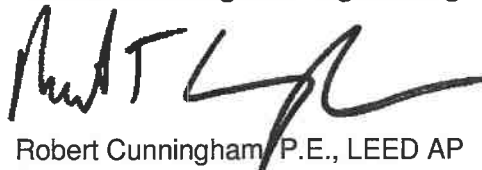
- c. Revise the stop bar detail to indicate the pavement marking details such as paint color and type.  
**Response: On Plan Sheet C1.2, the stop bar detail has been revised.**
- d. Revise the plans to show the full connection to the existing apartment building parking lot. The crosswalk location should be revised to tie-in to the proposed sidewalk with an ADA ramp.  
**Response: On Plan Sheet C1.1, the connection between the existing buildings (Phase I) and the proposed site is shown to tie in to the existing ADA spaces. Additionally, on plan sheet C2.2, spot elevations are provided.**
49. Revise the contact information on the cover sheet to be current (MCPC, MCCD, WWTP Engineer).  
**Response: On Plan Sheet C0.0, the cover sheet contact information has been revised.**
50. Revise the cover sheet title block to reflect total number of sheets (i.e. 1 of 19 sheets) in the plan set.  
**Response: The cover sheet has been updated to reflect the total number of sheets.**

Fire Marshal Review Letter from Ambler Borough dated January 22, 2026.

1. Section 507.5.1.1 of the 2021 International Fire Code requires that a fire hydrant be located within 100 feet of the fire department connection. Please provide an updated drawing that shows the proposed location of the required fire hydrant.  
**Response: A fire hydrant has been added to the plan.**

If you have any questions or require additional information, please do not hesitate to contact us at 215-586-3330 or [rob@hcengineering.net](mailto:rob@hcengineering.net)

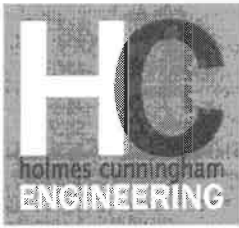
Very truly yours,  
**Holmes Cunningham Engineering**



Robert Cunningham, P.E., LEED AP  
Partner

Cc:

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Kristin Holmes, P.E., LEED AP  
Robert Cunningham, P.E., LEED AP

March 2, 2026

Borough of Ambler  
122 E. Butler Avenue  
Ambler, PA 19002

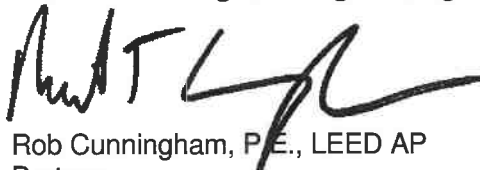
**Re: Ambler Crossings – Phase II, ELU Mid-Atlantic Ambler, LLC  
Waiver Request Letter  
TMP#: 01-00-01157-30-9  
Ambler Borough, Montgomery County, PA  
HCE Project No.: 1845**

Dear Borough Council:

In regard to the above referenced land development project, the applicant requests the following waivers from the Ambler Borough Code of Ordinances:

1. SALDO §22-308.C. – The applicant is requesting a waiver to allow the plan to be considered for simultaneous preliminary and final approval. The project has been extensively coordinated with the Township, such that there will be minimal difference between a preliminary and final plan.
2. SALDO §22-100.2.2.A – The applicant is requesting a waiver to allow more than 10 contiguous parking stalls in a row without a planting island. The proposed project and the surrounding uses utilize shared parking spaces to comply with parking requirements. Stormwater for the proposed impervious coverage is proposed to be managed in compliance with Borough requirements.
3. SALDO §22-100.4.3.G – The applicant is requesting a partial waiver from providing landscape buffering. The project site is adjoined to the northeast by an existing railroad, to the northwest by existing parking areas and a proposed retaining wall, and to the south by existing vegetation.

Very truly yours,  
**Holmes Cunningham Engineering**



Rob Cunningham, P.E., LEED AP  
Partner

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**November 14, 2025**

Peter Zaharchuk  
Summit Realty Advisors  
201 South Maple Street  
Suite 100  
Ambler, PA 19002

**RE: Parking Analysis – Ambler Crossings Phase II**

*Borough of Ambler, Montgomery County, PA*  
TPD No. SUMR.00073

Dear Pete,

As requested, TPD, Inc. has completed a Parking Analysis for the proposed Ambler Crossings Phase II development, located just south of the Existing Amber Crossings I Development, which is located at the southern terminus of South Maple Street and South Chestnut Street in the Borough of Ambler. The proposed site is located in the Redevelopment Overlay (RO) District and is considered a Transit Oriented District (TOD) by definition, due to its proximity to SEPTA's Ambler Train Station. The development will include 92 apartment units and will provide 138 off-street parking spaces, as shown in **Attachment A**. It should be noted that shared parking agreements exist between the existing Ambler Boiler House, the existing Ambler Crossings Phase I and the proposed development (Ambler Crossings Phase II). Between these three (3) properties, a total of 417 off-street parking spaces will be provided. An overall plan depicting the parking areas provided for these three (3) properties is shown in **Attachment B**.

The purpose of this letter is to evaluate the following:

- Determine the Parking Demand of the proposed development based on the Ambler Borough Zoning Ordinance requirements.
- Determine the Parking Demand of the proposed development utilizing ITE methodologies.
- Provide an opinion on the sufficiency of the parking proposed.

The following is a summary of TPD's findings:

**PARKING ANALYSIS**

*Borough of Ambler Zoning Requirements (Individual Uses)*

TPD evaluated the parking requirements for the proposed development based on the Zoning Requirements outlined in Section 27-2703.E(4)(e)2 (**Attachment C**), as shown in **Table 1**.

**TABLE 1**  
**BOROUGH OF AMBLER ZONING REQUIREMENTS**  
**INDIVIDUAL USES**

Use Type	Sub-Use	Section	Parking Requirement		
			Rate	X	Required Spaces
Residential	Apartments Studio/1-Bedroom <sup>1</sup>	27-2703.E(4)(e)2 (Table 2703.1)	1.0 spaces/du	67 du	67
	Apartments 2 or more Bedroom <sup>1</sup>		1.5 spaces/du	47 du	71
	Apartments Studio/1-Bedroom <sup>2</sup>		1.0 spaces/du	60 du	60
	Apartments 2 or more Bedroom <sup>2</sup>		1.5 spaces/du	32 du	48
Non-Residential	Professional Office <sup>3</sup>		1.0 space/300 sf GFA	36.400 ksf	122
<b>TOTAL</b>					<b>368</b>

*X = Independent Variable (du = dwelling units, ksf = 1000 sf)*

*1 = Ambler Crossings Phase I*

*2 = Ambler Crossings Phase II*

*3 = Ambler Boiler House*

Based on the Borough's Zoning Requirements shown in **Table 1**, the three (3) properties in question, will require a parking supply of **368 parking spaces**. The proposed parking supply of **417 parking spaces exceeds** the peak parking demands with a surplus of **49 parking spaces (+13.3%)**. Therefore, the proposed parking associated with the Ambler Boiler House, the existing Ambler Crossings Phase I and the proposed development (Ambler Crossings Phase II) will **exceed** the Township's Zoning requirements. *It should be noted that if only considering the proposed Ambler Crossings Phase II development, the proposed parking supply of **138 parking spaces** would exceed the Ordinance required parking of **108 parking spaces**.*

*Borough of Ambler Zoning Requirements (Shared Parking)*

Section 27-2703.E(4)(e)3 of the Borough's Ordinance allows a mixed-use development to share parking facilities, provided the shared parking area(s) are under common ownership or controlled by an access and parking easement agreement approved by the Borough and recorded for each property affected by the shared parking. Being that the three (3) properties in question are under common ownership and are controlled by access and parking easements, TPD evaluated the parking requirements assuming the sharing of parking, as shown in **Attachment D. Table 2** provides a summary of the results.

TABLE 2  
BOROUGH OF AMBLER ZONING PARKING REQUIREMENTS  
SHARED PARKING

Time of Day	Required Spaces
<b>Weekday</b>	
8:00 AM - 6:00 PM	<b>271</b>
6:00 PM – 12:00 AM	259
12:00 AM – 8:00 AM	253
<b>Weekend</b>	
8:00 AM - 6:00 PM	211
6:00 PM – 12:00 AM	253
12:00 AM – 8:00 AM	253

**Table 2** shows that when applying the Shared Parking methodologies outlined in the Borough’s Ordinance, the three (3) properties in question will require a parking supply of **271 parking spaces**. The proposed parking supply of **417 parking spaces exceeds** the peak parking demands with a surplus of **146 parking spaces (+53.9%)**. Therefore, the proposed parking associated with the Ambler Boiler House, the existing Ambler Crossings Phase I and the proposed development (Ambler Crossings Phase II) will **exceed** the Township’s Zoning requirements, when considering shared parking.

#### ITE Parking Generation

To determine the parking demand of the three (3) properties, TPD utilized Parking Demand Data and Time-of-Day Data from the ITE Parking Generation Manual (6<sup>th</sup> Edition), as included in **Attachment E**.

The parking data utilized is summarized in **Table 3**.

TABLE 3  
ITE PARKING GENERATION DATA

Land Use (ITE #)	Average Rate	
	Weekday	Weekend
Apartments (Mid-Rise) – Close to Rail (ITE# 221)	$P = 1.14*(X)$	$P = 1.34*(X)$
General Office (ITE# 710)	$P = 1.95*(X)$	$P = 0.20*(X)^1$

*P = Parking Demand; X = Independent Variable (du = dwelling units)*

*1 = No Weekend Data, Utilized 10% of weekday*

Based on the ITE Parking Demand data in **Table 3**, the peak parking demands of the three (3) properties, without time-of-day shared parking being considered, were determined to be **308 parking spaces** during a typical Weekday and **285 parking spaces** during a typical Weekend. These results are included in **Attachment F**. The proposed parking supply of **417 parking spaces exceeds** the peak parking demands without shared parking being considered during all time periods and provides for a minimum surplus of **109 parking spaces (+35.4%)**.

Based on the Time-of-Day Shared Parking Analysis, also contained in **Attachment F**, the peak parking demands of the three (3) properties were determined to be **237 parking spaces** during a typical Weekday

and **277 parking spaces** during a typical Weekend. The proposed parking supply of **417 parking spaces** **exceeds** the peak parking demands with time-of-day shared parking being considered during all time periods and provides for a minimum surplus of **140 parking spaces (+50.5%)**.

## CONCLUSIONS

**TPD has determined the following conclusions based on the findings in this letter:**

- **The three (3) properties (i.e. Ambler Boiler House, the existing Ambler Crossings Phase I and the proposed development (Ambler Crossings Phase II) provide a total parking supply of 417 off-street parking spaces.**
- **Based on the Borough's Zoning Requirements shown in Table 1, the proposed parking associated with the Ambler Boiler House, the existing Ambler Crossings Phase I and the proposed development (Ambler Crossings Phase II) will exceed the Township's Zoning requirements.**
- **It should be noted that if only considering the proposed Ambler Crossings Phase II development, the proposed parking supply of 138 parking spaces would exceed the Ordinance required parking of 108 parking spaces.**
- **Table 2 shows that when applying the Shared Parking methodologies outlined in the Borough's Ordinance, the proposed parking associated with the Ambler Boiler House, the existing Ambler Crossings Phase I and the proposed development (Ambler Crossings Phase II) will exceed the Township's Zoning requirements, when considering shared parking.**
- **Based on the ITE Parking Demand data in Table 3, the peak parking demands of the three (3) properties, without time-of-day shared parking being considered, exceed the peak parking demands during all time periods.**
- **Based on the ITE Time-of-Day Shared Parking Analysis, the peak parking demands of the three (3) properties exceed the peak parking demands during all time periods.**

**It is TPD's opinion that, based on the parking analysis prepared above, the parking supply proposed is sufficient to service not only the proposed Ambler Crossings Phase II development, but the three (3) mixed use properties discussed above.**

If you have any questions, or require any additional information, please do not hesitate to contact us.

Sincerely,

TPD



Matthew I. Hammond, P.E.  
Executive Vice President

Mhammond@TPDinc.com

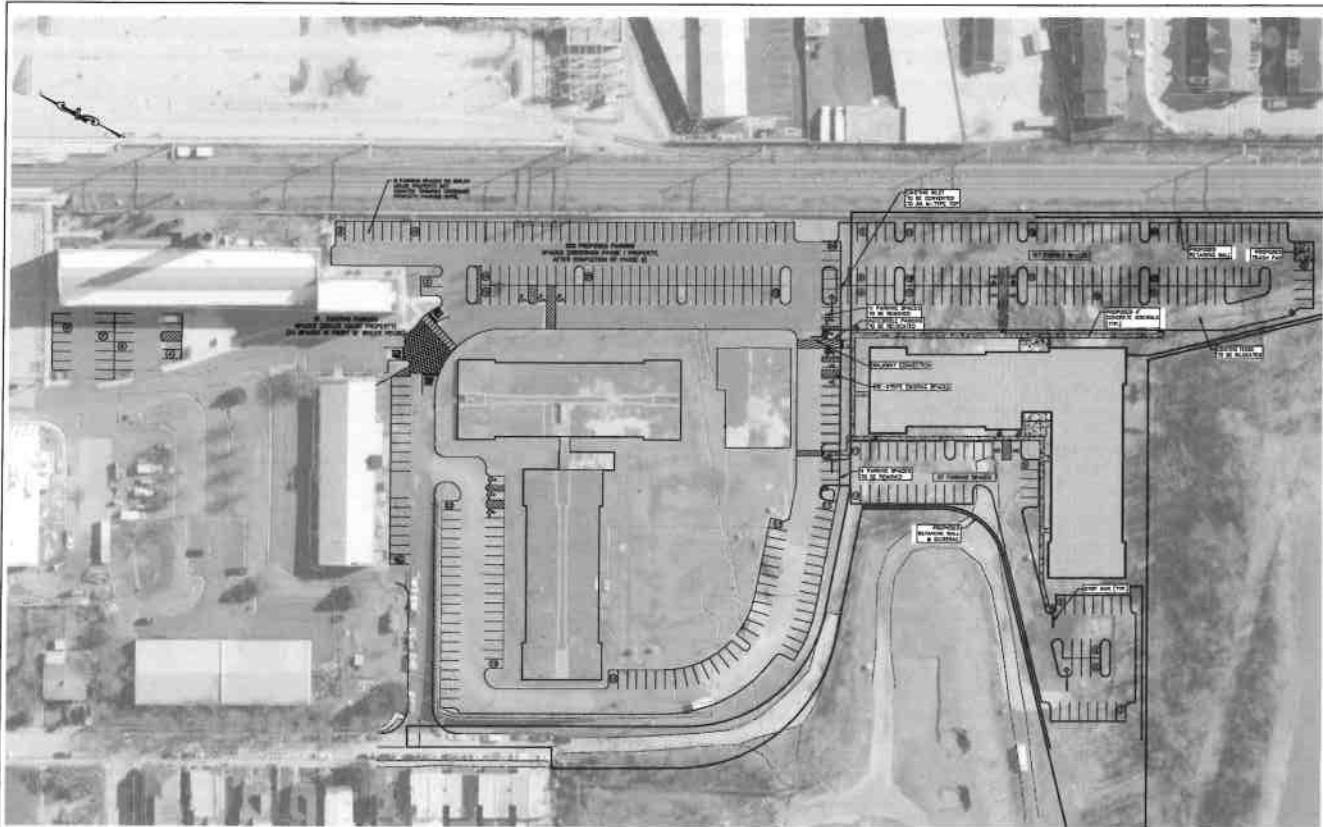
Attachments: Attachment A – Ambler Crossings Phase II Plan  
Attachment B – Overall Parking Plan  
Attachment C – Borough of Ambler Zoning Requirements  
Attachment D – Parking Analysis – Zoning Ordinance  
Attachment E – ITE Parking Generation Data  
Attachment F – Parking Analysis – ITE

cc: Ambler Borough  
Project Team  
TPD File

**ATTACHMENT A:**  
**AMBLER CROSSINGS PHASE II PLAN**



**ATTACHMENT B:**  
**OVERALL PARKING PLAN**



HOBBS CONSULTANTS, LLC  
 1000 W. MARKET STREET, SUITE 200  
 CHESTER, PA 19381  
 PH: 610.336.8800  
 WWW.HOBBSCONSULTANTS.COM



PROJECT NO.	SP1.0
DATE	10/15/2014
SCALE	AS SHOWN
DRAWN BY	SP1.0
CHECKED BY	SP1.0
DATE	10/15/2014



**AMBLER CROSSINGS - PHASE II**  
 MAPLE AVENUE AND CHESTNUT AVENUE  
 AMBLER TOWNSHIP, MONTGOMERY COUNTY, PENNSYLVANIA  
 AREA PARKING PLAN



NO. OF SHEETS	12
DATE	10/15/2014
SCALE	AS SHOWN
DRAWN BY	SP1.0
CHECKED BY	SP1.0
DATE	10/15/2014

Drawing No. SP1.0

**ATTACHMENT C:**  
**BOROUGH OF AMBLER ZONING REQUIREMENTS**

- b) New Developments of 5,000 to 14,999 Gross Floor Area shall provide no less than one item from §2703-E(4)(d)4a) and b), as set forth below, and shall provide no less than a cumulative total of eight (8) points.
  - c) New Developments of 15,000 Gross Floor Area and over, as well as any new building exceeding three (3) stories in height shall provide no less than one item from §2703-E(4)(d)4a) and b), as set forth below, and shall provide no less than a cumulative total of twelve (12) points.
- 4) Street furnishing options and points. In order to allow for a flexible and adaptable streetscape design, the following options shall be allowable to fulfill the minimum requirements set forth in §2703-E(4)(d)3), above. Category A contains planting and greening elements. Category B includes street furnishings, amenities, and decorations.
- a) Category A: planting and greening options.
    - i) Hanging basket, one (1) point.
    - ii) Window box, two (2) points.
    - iii) In-ground planting area, two (2) points.
    - iv) Street planter, two (2) points
    - v) Roof garden or green roof, three (3) points.
    - vi) Green wall, three (3) points.
  - b. Category B: street furnishings, amenities, and decorations.
    - i) Bench, two (2) points.
    - ii) Bicycle rack, two (2) points.
    - iii) Trash and recycling receptacles (one of each), two (2) points.
    - iv) Public art, such as a mural or sculpture, three (3) points.
    - v) Public drinking fountain, three (3) points.
    - vi) Public restroom, three (3) points.
    - vii) Display fountain, three (3) points.
    - viii) Clock tower, three (3) points.

(e) Parking Requirements.

- 1) Compliance with §27-2101, General Provisions, §27-2105, Handicapped Parking, and §27-2106, Off-Street Loading, shall be complied with; however, all other sections of Part 21 (§§27-2102, -2103, and -2104) shall be superseded by the regulations of this section.
- 2) Parking requirements by land use. Due to the anticipated use of transit by residents and tenants of a TOD, a reduced parking requirement is appropriate. The minimum parking requirements based on land use are shown in Table 2703.1 below:

Land Use	Minimum Parking Requirement
----------	-----------------------------

Retail establishment for the sale of dry goods, variety and general merchandise, clothing, food, drugs, plants, furnishings or other household supplies, sale and repair of jewelry, watches, clocks, optical goods or musical, professional or scientific instruments.	One (1) parking space per 200 SF GFA on the first floor + one (1) parking space per 400 SF GFA on upper floors
Business or professional office or studio, bank or other financial institution, municipal use excluding dump, telephone central office, telegraph or other public utility office, passenger station for public transportation	One (1) parking space per 300 SF GFA
Restaurant, bar, tearoom, retail baker, confectionery or ice cream shops or places serving food or beverages.	One (1) parking space per 200 SF GFA
Personal service shop, including tailor, barber, beauty salon, shoe repair, dressmaking or other similar service.	One (1) parking space per 300 SF GFA
Indoor recreation or cultural facility, such as a bowling alley, theater, fitness center, or dance studio.	One (1) parking space per 300 SF GFA
Hotel or motel.	One (1) parking space per guest room + One (1) parking space per 800 SF of public meeting space
Business services establishments, including copy centers, retail printing and duplication services, computer rental and copying centers, mailbox rental and shipping, cartage, express, and parcel delivery services.	1/300 SF GFA
Artisanal manufacturing	One (1) parking space per 200 SF GFA on the first floor + one (1) parking space per 400 SF GFA on upper floors
Studio or One Bedroom Dwelling Unit	One parking space per dwelling unit
Two or More Bedroom Dwelling Unit	1.5 parking spaces per dwelling unit

Table 2703.1

- 3) Shared parking facilities. The off-street parking required for a mixed-use development may be provided in combined parking facilities, provided that the following standards are met:
- a) The shared parking area(s) shall either be under common ownership or controlled by an access and parking easement agreement approved by the borough and recorded for each property affected by the shared parking.
  - b) The minimum number of parking spaces required shall be calculated according to the following formula:
    - i) Multiply the minimum parking requirement for each individual use (as set forth in Table 2703.1, above, for each use) by the appropriate percentage (as set forth in Table 2703.2, below) for each of the six designated time periods and then add the resulting sums from each vertical column. The column total

having the highest total value is the minimum shared parking space requirement for that combination of land uses.

- ii) Calculate the minimum amount of parking required for each land use as if it were a separate use.
- iii) To determine peak parking requirements, multiply the minimum parking required for each proposed land use by the corresponding percentage in the table below for each of the six time periods.
- iv) Calculate the column total for each of the six time periods.
- v) The column (time period) with the highest value shall be the minimum shared parking requirement.

Category of Use*	Monday to Friday			Saturday & Sunday		
	8am - 6pm	6pm -12am	12am -8am	8am - 6pm	6pm -12am	12am -8am
Retail establishment for the sale of dry goods, variety and general merchandise, clothing, food, drugs, plants, furnishings or other household supplies, sale and repair of jewelry, watches, clocks, optical goods or musical, professional or scientific instruments.	70%	90%	5%	100%	70%	5%
Business or professional office or studio, bank or other financial institution, municipal use excluding dump, telephone central office, telegraph or other public utility office, passenger station for public transportation.	100%	10%	5%	10%	5%	5%
Restaurant, bar, tearoom, retail baker, confectionery or ice cream shops or places serving food or beverages.	60%	100%	10%	60%	100%	20%
Personal service shop, including tailor, barber, beauty salon, shoe repair,	70%	90%	5%	100%	70%	5%

dressmaking or other similar service.						
Indoor recreation or cultural facility, such as a bowling alley, theater, fitness center, or dance studio.	40%	100%	10%	80%	100%	50%
Hotel/Motel	75%	100%	100%	75%	100%	100%
Business services establishments, including copy centers, retail printing and duplication services, computer rental and copying centers, mailbox rental and shipping, cartage, express, and parcel delivery services.	70%	90%	5%	100%	70%	5%
Artisanal manufacturing.	70%	90%	5%	100%	70%	5%
<b>Residential use</b>	<b>60%</b>	<b>100%</b>	<b>100%</b>	<b>80%</b>	<b>100%</b>	<b>100%</b>

\*NOTE: the most appropriate category of use shall be selected based upon the proposed use. The category of use shall fall within one of the uses permitted

- 4) Parking Study. In order to ensure that the parking provided for a TOD is adequate to meet the anticipated demand, a Parking Generation Study prepared and submitted by the applicant. The following provisions shall be met:
  - a) The Parking Generation Study shall be prepared by a qualified traffic engineer and/or transportation planner. All costs associated with the preparation of a Parking Generation Study shall be borne by the applicant. The study shall be conducted by a qualified transportation professional using the Institute of Transportation Engineers (ITE) Trip Generation Manual, 11th Edition, and the ITE Parking Generation Manual, 6th Edition, as applicable.
  - b) The Borough Engineer shall be provided an opportunity to review the proposed ITE land use codes selected by the transportation professional.
  - c) The study shall address the following:
    - i) Size and type of uses or activities on site;
    - ii) Composition of tenancy on site;
    - iii) Rate of parking turnover;
    - iv) Anticipated peak traffic and parking load;
    - v) Local parking habits; and
    - vi) Availability of public transportation.

- d) The findings of the study shall be submitted for review by the Borough Engineer; however, the study shall be considered complete and accurate unless proven otherwise by substantial evidence. In the event that the Parking Generation Study reveals a parking demand that differs significantly from the parking requirements of this section, the following options are available to the applicant:
  - i) If the anticipated parking demand exceeds the minimum parking requirement, the applicant shall be permitted the ability to provide up to the amount of parking spaces identified that exceed the minimum requirement.
  - ii) If the anticipated parking demand is lower than the minimum parking requirement, the applicant may have the reduced parking requirement authorized as a conditional use pursuant to §27-413, Conditional Uses.
- e) In lieu of a standalone parking generation study, the applicant may instead opt to include parking generation figures within the Transportation Impact Study required by §27-2705.C., Transportation Impact Study.

- (f) Gathering space. Any TOD lot that exceeds 150 feet in frontage along Main Street or Butler Avenue shall provide no less than 5% of the Transit-Oriented Development Effective Tract Area as a gathering space designed as a pedestrian plaza, courtyard, square, or pocket park. Furthermore, the following provisions shall be met:
  - 1) The gathering space shall be integral to the development and designed as a focal point for the TOD.
  - 2) The gathering space shall be located at street level, and shall be located adjacent to the sidewalk.
  - 3) The gathering space shall be convenient and accessible by sidewalk or internal pedestrian path.
  - 4) No fence or wall shall be erected between the gathering space and the sidewalk.
  - 5) At minimum, two (2) benches, one (1) bicycle rack, one (1) trash receptacle, and one (1) recycling receptacle shall be provided for every 2,500 square feet of gathering space.
  - 6) A minimum of 30%, but no more than 70%, of the gathering space shall be permeable planting beds landscaped with a combination of trees, shrubs, perennials, grasses, and groundcovers that provide year-round visual interest and color.
  - 7) The gathering space shall provide shade by using one or more of the following elements: canopies, trellises, umbrellas, or similar elements.
  - 8) Up to 25% of the gathering space area may be used for natural features preservation or stormwater management, provided the stormwater management facilities are designed as a vegetated amenity, such as rain gardens.

**ATTACHMENT D:**  
**PARKING ANALYSIS – ZONING ORDINANCE**

TPD# SUMR.00073

**SALDO PARKING REQUIREMENTS - NO SHARED PARKING**

Section 27-2703.E(4)(e)2 - Table 2703.1

<u>Use</u>	<u>Sub-Use</u>	<u>Development</u>	<u>Size</u>	<u>Parking Rate</u>	<u>Required Supply</u>
Residential	Apartments - Studio/1 BR	Ambler Crossing Phase I	67 du	1.00	67
Residential	Apartments - 2+ BR	Ambler Crossing Phase I	47 du	1.50	71
Residential	Apartments - Studio/1 BR	Ambler Crossing Phase II	60 du	1.00	60
Residential	Apartments - 2+ BR	Ambler Crossing Phase II	32 du	1.50	48
Non-Residential	Professional Office	Ambler Boiler House	36.400 ksf	3.33	122
<b><u>TOTAL PARKING REQUIREMENT =</u></b>					<b>368</b>

TPD# SUMR.00073  
 SALDO PARKING REQUIREMENTS - WITH SHARED PARKING  
 Section 27-2703.E(4)(e)2 - Table 2703.1

TIME OF DAY	Ambler Crossing Phase I - Apartments - Studio/1 BR											Ambler Crossing Phase I - Apartments - 2+ BR											Ambler Crossing Phase II - Apartments - Studio/1 BR											Ambler Crossing Phase II - Apartments - 2+ BR											Ambler Boiler House - Professional Office											Ambler Crossing Phase I - Apartments - Studio/1 BR											Ambler Crossing Phase I - Apartments - 2+ BR											Ambler Crossing Phase II - Apartments - Studio/1 BR											Ambler Crossing Phase II - Apartments - 2+ BR											Ambler Boiler House - Professional Office											TOTAL
	WEEKDAY (MON-FRI)													WEEKEND (SAT-SUN)																																																																																																	
8:00am - 6:00pm	60%	60%	60%	60%	100%	41	43	36	29	122	271	80%	80%	80%	80%	10%	54	57	48	39	13	211	<<<WEEKDAY PEAK																																																																																								
6:00pm - 12:00am	100%	100%	100%	100%	10%	67	71	60	48	13	259	100%	100%	100%	100%	5%	67	71	60	48	7	253	<<<WEEKEND PEAK																																																																																								
12:00am - 8:00am	100%	100%	100%	100%	5%	67	71	60	48	7	253	100%	100%	100%	100%	5%	67	71	60	48	7	253	<<<WEEKEND PEAK																																																																																								

**ATTACHMENT E:**  
**ITE PARKING GENERATION DATA**



A Community of Transportation Professionals

Institute of Transportation Engineers



# Parking Generation Manual

6th Edition

October 2023

## Land Use: 221 Multifamily Housing— 2+ BR (Mid-Rise)

### Description

Mid-rise multifamily housing with two or more bedrooms is a residential building with between four and 10 floors (levels) of residence that contain at least one dwelling unit with two or more bedrooms. Access to individual dwelling units is through an outside building entrance, a lobby, elevator, and a set of hallways.

### Land Use Subcategory

Data are separated into two subcategories for this land use: (1) not close to rail transit and (2) close to rail transit. A site is considered close to rail transit if the walking distance between the residential site entrance and the closest rail transit station entrance is ½ mile or less.

### Time-of-Day Distribution for Parking Demand

The following table presents a composite (weekday and Saturday) Time-of-Day distribution of parking demand for three general urban/suburban study sites.

Hour Beginning	Percent of Peak Parking Demand
	Weekday/Saturday Composite
12:00-4:00 a.m.	100
5:00 a.m.	96
6:00 a.m.	86
7:00 a.m.	77
8:00 a.m.	66
9:00 a.m.	60
10:00 a.m.	57
11:00 a.m.	55
12:00 p.m.	52
1:00 p.m.	50
2:00 p.m.	52
3:00 p.m.	51
4:00 p.m.	57
5:00 p.m.	62
6:00 p.m.	65
7:00 p.m.	68
8:00 p.m.	75
9:00 p.m.	82
10:00 p.m.	87
11:00 p.m.	91

## Additional Data

The average parking supply ratios and average peak parking occupancy for the study sites with parking supply information are shown in the table below.

Setting	Proximity to Rail Transit	Parking Supply Per Dwelling Unit	Average Peak Parking Occupancy
Center City Core	Within ½ mile of rail transit	0.73 (8 sites)	69%
Dense Multi-Use Urban	Within ½ mile of rail transit	0.88 (31 sites)	81%
	Not within ½ mile of rail transit	1.1 (35 sites)	76%
General Urban/Suburban	Within ½ mile of rail transit	1.5 (6 sites)	74%
	Not within ½ mile of rail transit	1.7 (38 sites)	72%

The sites were surveyed in the 1990s, the 2000s, the 2010s, and the 2020s in California, Connecticut, District of Columbia, Maine, Maryland, Massachusetts, North Carolina, Ontario (CAN), Oregon, Tennessee, Virginia, Washington, and Wisconsin.

## Source Numbers

209, 255, 277, 402, 419, 505, 512, 533, 535, 536, 537, 545, 546, 547, 575, 576, 577, 579, 581, 583, 584, 585, 587, 602, 603, 604, 620, 631

# Multifamily Housing - 2+ BR (Mid-Rise) Close to Rail Transit (221)

Peak Period Parking Demand vs: Dwelling Units

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

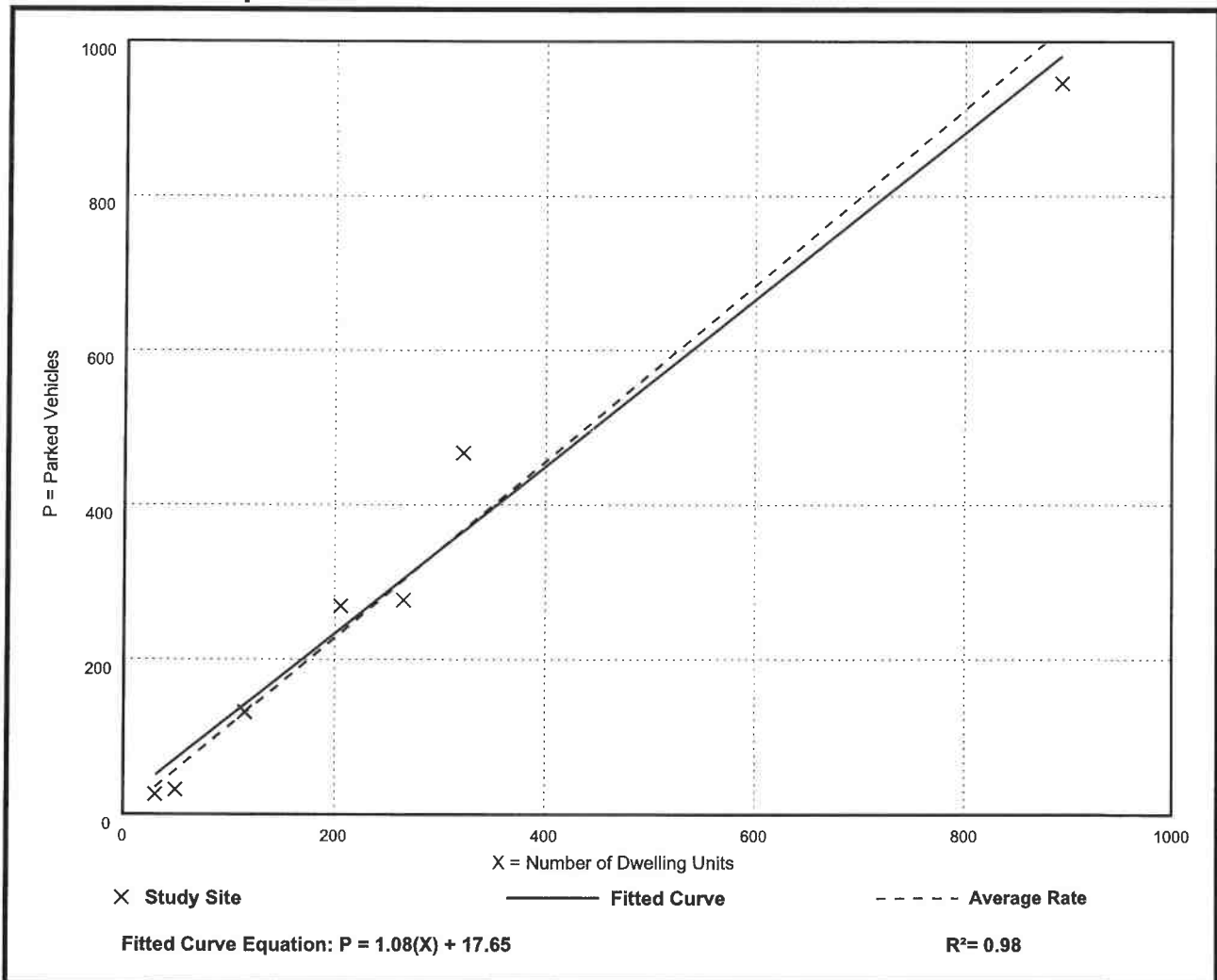
Number of Studies: 7

Avg. Num. of Dwelling Units: 269

## Peak Period Parking Demand per Dwelling Unit

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
1.14	0.64 - 1.45	0.97 / 1.42	***	0.19 ( 17% )

## Data Plot and Equation



# Multifamily Housing - 2+ BR (Mid-Rise) Close to Rail Transit (221)

Peak Period Parking Demand vs: Dwelling Units

On a: Saturday

Setting/Location: General Urban/Suburban

Number of Studies: 2

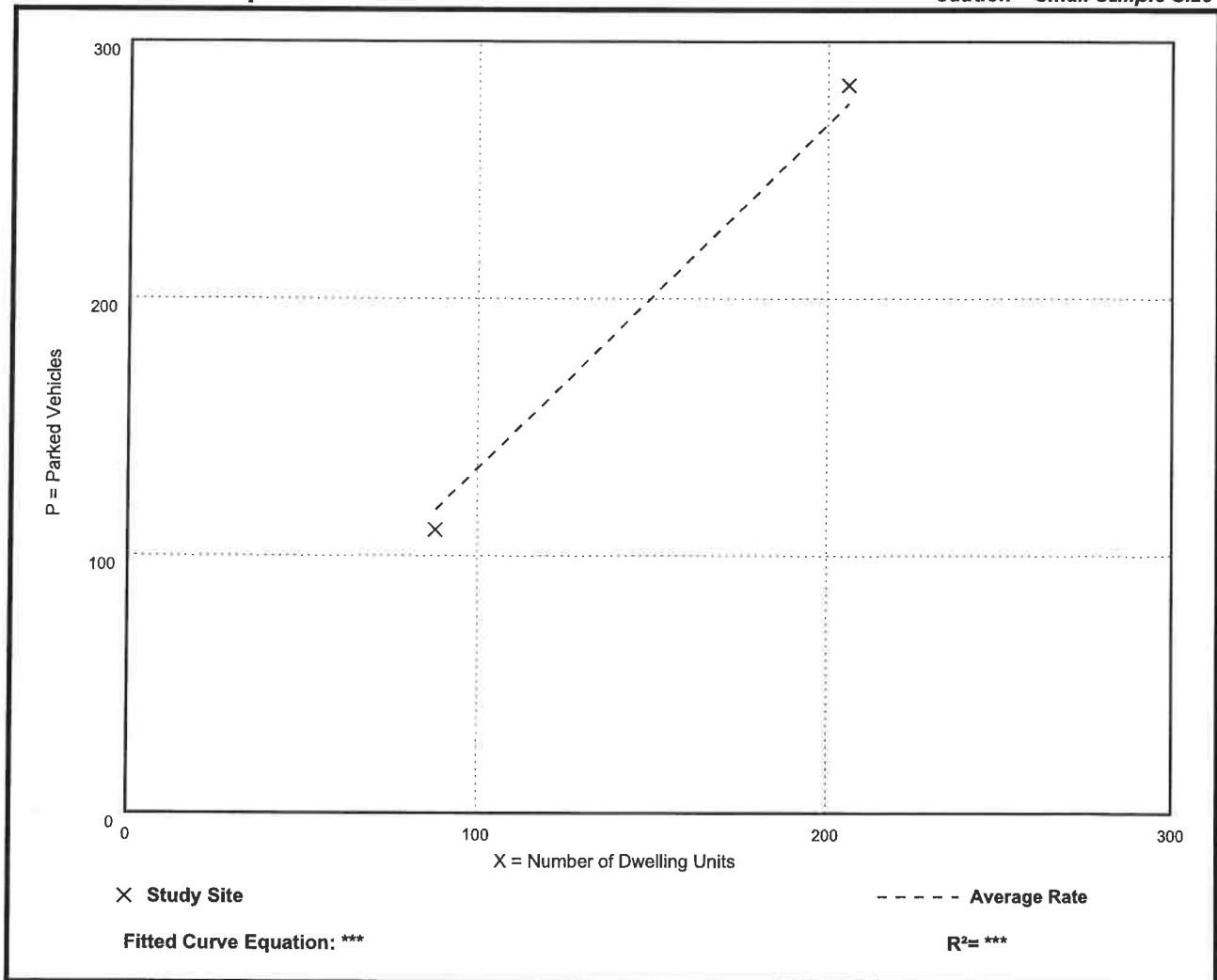
Avg. Num. of Dwelling Units: 147

## Peak Period Parking Demand per Dwelling Unit

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
1.34	1.25 - 1.37	*** / ***	***	*** ( ***)

## Data Plot and Equation

Caution – Small Sample Size



## Land Use: 710 General Office Building

### Description

A general office building is a building with multiple tenants that employ persons in the management, direction, or conduct of legal, accounting, engineering, consulting, real estate, insurance, financial, or other professional services. A general office building with a gross floor area of 10,000 square feet or less is classified as a small office building (Land Use 712).

### Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a weekday at 19 study sites in a general urban/suburban setting.

Hour Beginning	Percent of Weekday Peak Parking Demand	
	General Urban/Suburban	
12:00-4:00 a.m.	—	
5:00 a.m.	—	
6:00 a.m.	—	
7:00 a.m.	13	
8:00 a.m.	47	
9:00 a.m.	87	
10:00 a.m.	99	
11:00 a.m.	100	
12:00 p.m.	86	
1:00 p.m.	84	
2:00 p.m.	93	
3:00 p.m.	93	
4:00 p.m.	85	
5:00 p.m.	57	
6:00 p.m.	21	
7:00 p.m.	—	
8:00 p.m.	—	
9:00 p.m.	—	
10:00 p.m.	—	
11:00 p.m.	—	

## **Additional Data**

For the seven study sites with parking supply information and located in a dense multi-use urban setting, the average parking supply ratio is 2.9 spaces per 1,000 square feet GFA. At these sites, the average peak parking occupancy is 56 percent.

For the 63 study sites with parking supply information and located in a general urban/suburban setting, the average parking supply ratio is 3.3 spaces per 1,000 square feet GFA. At these sites, the average peak parking occupancy is 60 percent.

For nine study sites, parking demand data were collected on a Saturday as well as a weekday. For those sites, peak Saturday parking demand averages 13 percent of the peak weekday parking demand.

The sites were surveyed in the 1990s, the 2000s, the 2010s, and the 2020s in Alabama, Arizona, California, Colorado, District of Columbia, Georgia, Illinois, Indiana, Maine, Maryland, Massachusetts, Minnesota, Montana, New Jersey, New York, Ontario (CAN), Oregon, Pennsylvania, Texas, Utah, Virginia, Washington, and Wisconsin.

## **Source Numbers**

122, 201, 211, 217, 276, 425, 431, 433, 436, 438, 440, 516, 531, 540, 551, 555, 556, 567, 571, 572, 588, 607, 618, 622, 633

# General Office Building (710)

**Peak Period Parking Demand vs: 1000 Sq. Ft. GFA**

**On a: Weekday (Monday - Friday)**

**Setting/Location: General Urban/Suburban**

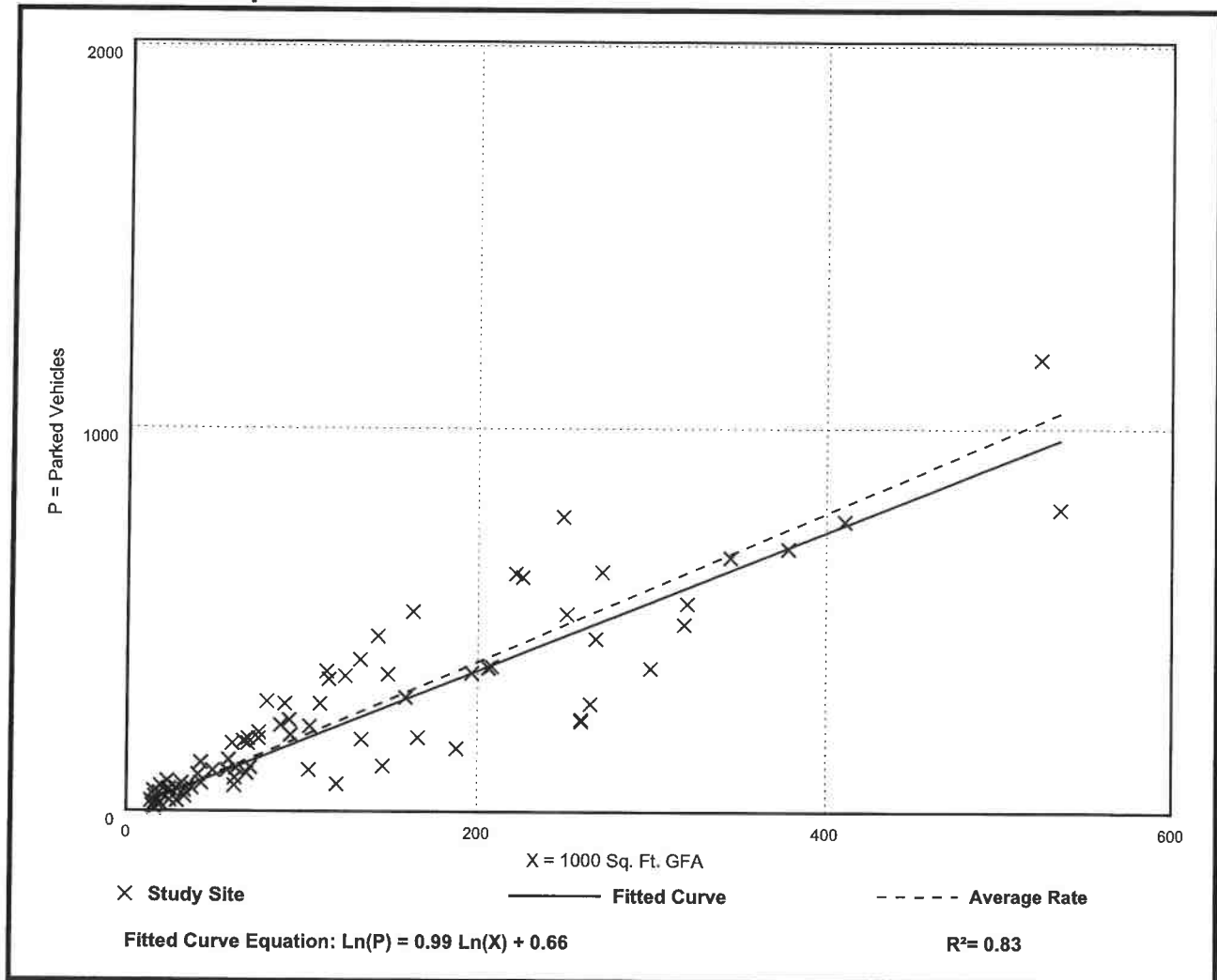
Number of Studies: 77

Avg. 1000 Sq. Ft. GFA: 131

## Peak Period Parking Demand per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
1.95	0.50 - 3.60	1.68 / 2.98	1.79 - 2.11	0.70 ( 36% )

## Data Plot and Equation



**ATTACHMENT F:**  
**PARKING ANALYSIS - ITE**

TPD# SUMR.00073  
 ITE PARKING DEMAND - NO SHARED PARKING  
 ITE Parking Manual - 6th Edition

WEEKDAY

<u>Use</u>	<u>ITE#</u>	<u>Land Use</u>	<u>Size</u>		<u>Notes</u>	<u>Rate (50ft)</u>	<u>Demand (50ft)</u>
Ambler Crossing Phase I	221	Apartments (Mid-Rise)	67	du	Close to Rail Transit	1.14	77
Ambler Crossing Phase I	221	Apartments (Mid-Rise)	47	du	Close to Rail Transit	1.14	54
Ambler Crossing Phase II	221	Apartments (Mid-Rise)	60	du	Close to Rail Transit	1.14	69
Ambler Crossing Phase II	221	Apartments (Mid-Rise)	32	du	Close to Rail Transit	1.14	37
Ambler Boiler House	710	General Office	36.400	ksf		1.95	71
<b>Peak Weekday Parking Demand (No Shared Parking)</b>							<b>308</b>

WEEKEND

<u>Use</u>	<u>ITE#</u>	<u>Land Use</u>	<u>Size</u>		<u>Notes</u>	<u>Rate (50ft)</u>	<u>Demand (50ft)</u>
Ambler Crossing Phase I	221	Apartments (Mid-Rise)	67	du	Close to Rail Transit	1.34	90
Ambler Crossing Phase I	221	Apartments (Mid-Rise)	47	du	Close to Rail Transit	1.34	63
Ambler Crossing Phase II	221	Apartments (Mid-Rise)	60	du	Close to Rail Transit	1.34	81
Ambler Crossing Phase II	221	Apartments (Mid-Rise)	32	du	Close to Rail Transit	1.34	43
Ambler Boiler House	710	General Office	36.400	ksf	No ITE Data (Assumed 10% of Weekday Similar to SALDO % Comparison)	0.20	8
<b>Peak Weekend Parking Demand (No Shared Parking)</b>							<b>285</b>

TPD# SUMR.00073  
 ITE PARKING DEMAND - SHARED PARKING  
 WEEKDAY  
 ITE Parking Manual - 6th Edition

TIME OF DAY	Ambler Crossing Phase I - Apartments - Studio/1 BR	Ambler Crossing Phase I - Apartments - 2+ BR	Ambler Crossing Phase II - Apartments - Studio/1 BR	Ambler Crossing Phase II - Apartments - 2+ BR	Ambler Boiler House - Professional Office	Ambler Crossing Phase I - Apartments - Studio/1 BR	Ambler Crossing Phase I - Apartments - 2+ BR	Ambler Crossing Phase II - Apartments - Studio/1 BR	Ambler Crossing Phase II - Apartments - 2+ BR	Ambler Boiler House - Professional Office	TOTAL	
12:00am	100%	100%	100%	100%	0%	77	54	69	37	0	237	<<<WEEKDAY PEAK
1:00am	100%	100%	100%	100%	0%	77	54	69	37	0	237	<<<WEEKDAY PEAK
2:00am	100%	100%	100%	100%	0%	77	54	69	37	0	237	<<<WEEKDAY PEAK
3:00am	100%	100%	100%	100%	0%	77	54	69	37	0	237	<<<WEEKDAY PEAK
4:00am	100%	100%	100%	100%	0%	77	54	69	37	0	237	<<<WEEKDAY PEAK
5:00am	96%	96%	96%	96%	0%	74	52	67	36	0	229	
6:00am	86%	86%	86%	86%	0%	67	47	60	32	0	206	
7:00am	77%	77%	77%	77%	13%	60	42	54	29	10	195	
8:00am	66%	66%	66%	66%	47%	51	36	46	25	34	192	
9:00am	60%	60%	60%	60%	87%	47	33	42	23	62	207	
10:00am	57%	57%	57%	57%	99%	44	31	40	22	71	208	
11:00am	55%	55%	55%	55%	100%	43	30	38	21	71	203	
12:00pm	52%	52%	52%	52%	86%	41	29	36	20	62	188	
1:00pm	50%	50%	50%	50%	84%	39	27	35	19	60	180	
2:00pm	52%	52%	52%	52%	93%	41	29	36	20	67	193	
3:00pm	51%	51%	51%	51%	93%	40	28	36	19	67	190	
4:00pm	57%	57%	57%	57%	85%	44	31	40	22	61	198	
5:00pm	62%	62%	62%	62%	57%	48	34	43	23	41	189	
6:00pm	65%	65%	65%	65%	21%	51	36	45	25	15	172	
7:00pm	68%	68%	68%	68%	0%	53	37	47	26	0	163	
8:00pm	75%	75%	75%	75%	0%	58	41	52	28	0	179	
9:00pm	82%	82%	82%	82%	0%	64	45	57	31	0	197	
10:00pm	87%	87%	87%	87%	0%	67	47	61	33	0	208	
11:00pm	91%	91%	91%	91%	0%	71	50	63	34	0	218	

TPD# SUMR.00073  
 ITE PARKING DEMAND - SHARED PARKING  
 WEEKEND  
 ITE Parking Manual - 6th Edition

TIME OF DAY	Ambler Crossing Phase I - Apartments - Studio/1 BR	Ambler Crossing Phase I - Apartments - 2+ BR	Ambler Crossing Phase II - Apartments - Studio/1 BR	Ambler Crossing Phase II - Apartments - 2+ BR	Ambler Boiler House - Professional Office	Ambler Crossing Phase I - Apartments - Studio/1 BR	Ambler Crossing Phase I - Apartments - 2+ BR	Ambler Crossing Phase II - Apartments - Studio/1 BR	Ambler Crossing Phase II - Apartments - 2+ BR	Ambler Boiler House - Professional Office	TOTAL	
12:00am	100%	100%	100%	100%	0%	90	63	81	43	0	277	<<<WEEKEND PEAK
1:00am	100%	100%	100%	100%	0%	90	63	81	43	0	277	<<<WEEKEND PEAK
2:00am	100%	100%	100%	100%	0%	90	63	81	43	0	277	<<<WEEKEND PEAK
3:00am	100%	100%	100%	100%	0%	90	63	81	43	0	277	<<<WEEKEND PEAK
4:00am	100%	100%	100%	100%	0%	90	63	81	43	0	277	<<<WEEKEND PEAK
5:00am	96%	96%	96%	96%	0%	87	61	78	42	0	268	
6:00am	86%	86%	86%	86%	0%	78	55	70	37	0	240	
7:00am	77%	77%	77%	77%	13%	70	49	63	34	2	218	
8:00am	66%	66%	66%	66%	47%	60	42	54	29	4	189	
9:00am	60%	60%	60%	60%	87%	54	38	49	26	7	174	
10:00am	57%	57%	57%	57%	99%	52	36	47	25	8	168	
11:00am	55%	55%	55%	55%	100%	50	35	45	24	8	162	
12:00pm	52%	52%	52%	52%	86%	47	33	43	23	7	153	
1:00pm	50%	50%	50%	50%	84%	45	32	41	22	7	147	
2:00pm	52%	52%	52%	52%	93%	47	33	43	23	8	154	
3:00pm	51%	51%	51%	51%	93%	46	33	42	22	8	151	
4:00pm	57%	57%	57%	57%	85%	52	36	47	25	7	167	
5:00pm	62%	62%	62%	62%	57%	56	40	51	27	5	179	
6:00pm	65%	65%	65%	65%	21%	59	41	53	28	2	183	
7:00pm	68%	68%	68%	68%	0%	62	43	56	30	0	191	
8:00pm	75%	75%	75%	75%	0%	68	48	61	33	0	210	
9:00pm	82%	82%	82%	82%	0%	74	52	67	36	0	229	
10:00pm	87%	87%	87%	87%	0%	79	55	71	38	0	243	
11:00pm	91%	91%	91%	91%	0%	82	58	74	40	0	254	